

GENERAL NOTES:

1. BASIS OF BEARING AND THE BASIS OF ELEVATION ARE PER TOPOGRAPHIC SURVEY DONE BY EXTREME MEASURES LAND SURVEYS, INC.
2. BASIS OF BEARING IS THE SOUTH LINE OF THE SCHOOL PROPERTY BOUNDARY.
3. BASIS OF ELEVATION IS KAY 16--COORDINATES PROVIDED BY DON JACKSON OF THE KAYENTA TOWNSHIP ENGINEERING DEPARTMENT.
4. A COPY OF THESE APPROVED PLANS SHALL BE KEPT IN AN EASILY ACCESSIBLE LOCATION ON THE CONSTRUCTION SITE AT ALL TIMES DURING CONSTRUCTION.
5. CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) REGULATIONS.
6. ALL MINOR OBSTRUCTIONS MAY NOT BE SHOWN ON PLANS. THE CONTRACTOR SHALL VISIT THE SITE AND DETERMINE THE ACTUAL CONDITIONS WITH REGARD TO THE EXISTENCE OF FENCES, DRIVEWAYS, TREES, SIDEWALKS, CULVERTS, UTILITIES AND OTHER MISCELLANEOUS OBSTACLES THAT MAY INTERFERE WITH CONSTRUCTION PRIOR TO SUBMITTING BIDS.
7. IF UNANTICIPATED SITE CONDITIONS ARE ENCOUNTERED DURING THE COURSE OF CONSTRUCTION OR OMISSIONS AND/OR CONFLICT BETWEEN VARIOUS ELEMENTS OF CONSTRUCTION DRAWINGS, NOTES, AND/OR DETAILS, THEY SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER OF RECORD IMMEDIATELY UPON BECOMING AWARE OF SUCH CONDITIONS, OMISSIONS AND/OR CONFLICTS AND WORK SHALL CEASE AND NOT PROCEED IN THE AREA OF CONCERN UNTIL AUTHORIZED BY THE ENGINEER OF RECORD.
8. THE INFORMATION REGARDING THE TYPE, SIZE, AND LOCATION OF UTILITIES IS SHOWN BASED ON BEST INFORMATION AVAILABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING EXACT LOCATIONS AND ELEVATIONS OF ALL UTILITIES FROM THE OWNERS OF RESPECTIVE UTILITIES. ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 72 HOURS PRIOR TO EXCAVATION. CONTRACTOR SHALL NOTIFY THE OWNER OR HIS AUTHORIZED REPRESENTATIVE OF ANY DISCREPANCIES PRIOR TO PROCEEDING WITH WORK AND SHALL PROTECT ALL EXISTING UTILITIES THROUGH THE DURATION OF CONSTRUCTION. NO ASSURANCE IS HEREBY GIVEN THAT ALL EASEMENTS OR UTILITIES ARE COMPLETELY OR ACCURATELY DEPICTED.
9. ANY AMBIGUITIES OR DEFICIENCIES DISCOVERED ON THESE PLANS ARE TO BE RESOLVED BY ARROWHEAD ENGINEERING, INC.. ANYONE ELSE WHO MODIFIES THE PLANS OR THE DESIGNS ON THEM OR RESOLVES AMBIGUITIES OR DEFICIENCIES BECOMES RESPONSIBLE FOR HIS OR HER MODIFICATIONS OR DECISION.
10. UTILITY INFORMATION SHOWN ON THESE PLANS ARE PROVIDED BY NTUA GIS AND CONTRACTOR MUST VERIFY. THE ENGINEER MAKES NO GUARANTEE REGARDING THE LOCATION OR ELEVATION OF EXISTING UTILITIES SHOWN ON THESE PLANS. THE CONTRACTOR SHALL CALL BLUE STAKE FOR ALL UTILITIES.
11. CONTRACTOR SHALL GIVE A MINIMUM OF FORTY-EIGHT (48) HOURS NOTICE BEFORE HE REQUIRES HORIZONTAL OR VERTICAL LAYOUT OF ANY PORTION OF THE WORK. HE SHALL ALSO DIG ALL STAKE HOLES NECESSARY TO GIVE LINES AND LEVELS. MEASUREMENTS OF THE WORK SHALL REMAIN IN THEIR PROPER PLACES UNTIL AUTHORIZED TO REMOVE THEM BY THE ENGINEER OF RECORD OR HIS AGENT. ANY EXPENSE INCURRED IN REPLACING ANY STAKES THAT THE CONTRACTOR OR HIS SUBORDINATES MAY HAVE FAILED TO PRESERVE SHALL BE CHARGED TO THE CONTRACTOR.
12. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO FURNISH, HAUL, AND APPLY ALL WATER REQUIRED FOR COMPACTION AND FOR THE CONTROL OF DUST FROM CONSTRUCTION ACTIVITY. THE COST THEREOF IS TO BE INCLUDED IN THE CONSTRUCTION PRICE.
13. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HAVE A REGISTERED SOILS ENGINEER OBSERVE, INSPECT, AND TEST ALL EARTHWORK PREPARATION. EARTHWORK PREPARATION IS REGARDED AS (BUT NOT LIMITED TO) STRUCTURAL AND TRENCH EXCAVATION, BACKFILL, AND TOGETHER WITH THE PLACEMENT OF FILL MATERIAL AND ITS SUBSEQUENT COMPACTION.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CARE, MAINTENANCE, REPAIR OR REPLACEMENT OF ANY EXISTING IMPROVEMENT IN THE WORK AREA WHICH BECOMES DAMAGED OR IS REMOVED DURING THE COURSE OF CONSTRUCTION. ALL REPAIR, REPLACEMENT, OR CLEANUP SHALL BE DONE TO THE SATISFACTION OF THE OWNER.
15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DEVELOPING A TEMPORARY TRAFFIC CONTROL PLAN THAT MEETS THE REQUIREMENTS OF THE MUTCD (LATEST EDITION WITH SUPPLEMENTS) AND THE OWNER'S AUTHORIZED REPRESENTATIVE. THE CONTRACTOR SHALL PROPERLY MAINTAIN THIS PLAN FOR ALL WORK WITHIN THE PROJECT LIMITS.
16. THE CONTRACTOR SHALL CHECK AND VERIFY ALL DIMENSIONS, GRADES AND POTHOLE EXISTING UTILITY LOCATIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION. ALL DISCREPANCIES AND DEVIATIONS FROM EXISTING SITE CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE DESIGN ENGINEER PRIOR TO START OF CONSTRUCTION.
17. QUANTITIES SHOWN THROUGHOUT THESE DRAWINGS AND SPECIFICATIONS ARE APPROXIMATE. THEY SHALL NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY FOR INDEPENDENTLY ESTIMATING BID OR WORK QUANTITIES.
18. ALL MATERIALS AND WORKMANSHIP SHALL COMPLY WITH THE CURRENT "MAG UNIFORM STANDARD SPECIFICATIONS AND DETAILS FOR PUBLIC WORKS CONSTRUCTION" AND WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICE. ALL WORK AND MATERIALS WHICH DO NOT CONFORM TO THE STANDARDS AND SPECIFICATIONS ARE SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.
19. ANY WORK PERFORMED WITHOUT THE KNOWLEDGE AND APPROVAL OF THE OWNER OR HIS AUTHORIZED REPRESENTATIVE IS SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.
20. THE OWNER OR HIS AUTHORIZED REPRESENTATIVE MAY SUSPEND THE WORK BY WRITTEN NOTICE WHEN, IN HIS JUDGMENT, PROGRESS IS UNSATISFACTORY, WORK BEING DONE IS UNAUTHORIZED OR DEFECTIVE, WEATHER CONDITIONS ARE UNSUITABLE, OR THERE IS DANGER TO THE PUBLIC HEALTH OR SAFETY.
21. THE OWNER MAY ORDER ANY AND ALL MATERIALS USED IN THE WORK TO BE TESTED ACCORDING TO THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS (AASHTO) AND THE AMERICAN SOCIETY OF TESTING AND MATERIALS (ASTM) STANDARDS. THE CONTRACTOR SHALL, AT HIS EXPENSE, SUPPLY ALL SAMPLES REQUIRED FOR TESTING.
22. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF THE PAVEMENT AND OF PARTIALLY COMPLETED PORTIONS OF THE WORK UNTIL FINAL ACCEPTANCE OF THE WORK. THE CONTRACTOR SHALL SUBMIT TO THE OWNER OR HIS AUTHORIZED REPRESENTATIVE FOR APPROVAL A CONSTRUCTION SCHEDULE FOR ANY STREETS REQUIRED TO BE CLOSED OR PARTIALLY CLOSED FOR THE CONSTRUCTION ACTIVITY. THE CONTRACTOR SHALL REOPEN THE STREETS NO LATER THAN THE OPENING DATE SHOWN ON THE CONSTRUCTION SCHEDULE OR UPON ORDER OF THE OWNER OR HIS AUTHORIZED REPRESENTATIVE. THE REGULATION AND CONTROL OF CONSTRUCTION TRAFFIC SHALL BE AS DIRECTED BY THE OWNER OR HIS AUTHORIZED REPRESENTATIVE.
23. APPROVAL OF A PORTION OF THE WORK IN PROGRESS DOES NOT GUARANTEE ITS FINAL ACCEPTANCE. TESTING AND EVALUATION MAY CONTINUE UNTIL WRITTEN FINAL ACCEPTANCE OF A COMPLETE WORKABLE UNIT. ANY DEFECTS WHICH APPEAR IN THE WORK WITHIN ONE YEAR FROM THE DATE OF ACCEPTANCE AND WHICH ARE DUE TO IMPROPER WORKMANSHIP OR INFERIOR MATERIALS SUPPLIED SHALL BE CORRECTED BY OR AT THE EXPENSE OF THE CONTRACTOR.
24. ACCEPTANCE OF COMPLETED IMPROVEMENTS WILL NOT BE GIVEN UNTIL DEFECTIVE OR UNAUTHORIZED WORK IS REMOVED, AND FINAL CLEAN-UP IS COMPLETE.
25. LOCATION OF UNDERGROUND UTILITIES BEFORE WORK IS BEGUN IS TO BE ACCOMPLISHED IN ACCORDANCE WITH ARS 40-360.22.
26. THE ESTABLISHMENT AND USE OF TEMPORARY CONSTRUCTION YARDS SHALL RECEIVE PRIOR APPROVAL FROM THE OWNER OR HIS AUTHORIZED REPRESENTATIVE.
27. ALL EXCAVATED MATERIAL SHALL BE DISPOSED OF IN ACCORDANCE WITH APPLICABLE KAYENTA TOWNSHIP CODES AND REGULATIONS. THE CONTRACTOR SHALL OBTAIN ALL REQUIRED KAYENTA TOWNSHIP'S APPROVALS AND PERMITS, AS DEEMED NECESSARY BY THE KAYENTA TOWNSHIP, TO DISPOSE OF EXCAVATED MATERIAL.
28. ALL CONSTRUCTION STAKING SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND PERFORMED UNDER THE DIRECT SUPERVISION OF A REGISTERED LAND SURVEYOR OR CIVIL ENGINEER.

EARTHWORK:

1. CONTRACTOR SHALL FOLLOW RECOMMENDATIONS FROM ATEK ENGINEERING CONSULTANTS, LLC GEOTECHNICAL EXPLORATION REPORT, KAYENTA BUS ROUTE PHASE 1, KAYENTA ARIZONA, PROJECT NO. 170074, DATED 6-2-17.
2. GENERAL - THE CONTRACTOR SHALL ACCEPT REASONABLE VARIATIONS IN SITE CONDITIONS. NO EXTRA PAYMENT SHALL BE ALLOWED FOR REMEDIATION OF SUBGRADE MATERIALS DUE TO HIGH MOISTURE CONTENT OR POCKETS OF OVERLY PLASTIC MATERIALS WHICH CAN BE REMEDIATED THROUGH MIXING WITH ON SITE MATERIALS, AS DETERMINED BY THE ENGINEER. IF REMOVAL AND REPLACEMENT OF MATERIAL IS REQUIRED, A CHANGE ORDER WILL BE ISSUED AT THE DISCRETION OF THE OWNER, WITH PAYMENT BASED UPON PREDETERMINED UNIT COSTS.
3. NO FILL SHALL BE PLACED OVER FROZEN GROUND OR ON-SITE CLAY. IMPORTED AND ON-SITE SOIL SHALL BE COMPACTED WITHIN A WATER CONTENT RANGE OF 3 PERCENT BELOW TO 3 PERCENT ABOVE OPTIMUM WATER CONTENT.
4. SUBGRADE PREPARATION - SCARIFY, PREPARE TO PROPER MOISTURE CONTENT, AND RE-COMPACT TO A MINIMUM DEPTH OF 8 INCHES PRIOR TO PLACEMENT OF FILL AND ROAD MATERIALS.
5. IMPORT SOILS - IMPORTED SOILS SHALL CONFORM TO THE FOLLOWING IN ACCORDANCE WITH THE SOILS REPORT:

GRADATION (ASTM C136)	PERCENT FINER BY WEIGHT
SIEVE SIZE (% FINER BY WEIGHT)	(% PASSING)
3"	100
NO. 4 SIEVE	50-100
NO. 200 SIEVE	50 (MAX)
MAXIMUM SWELL POTENTIAL (%)	1.50

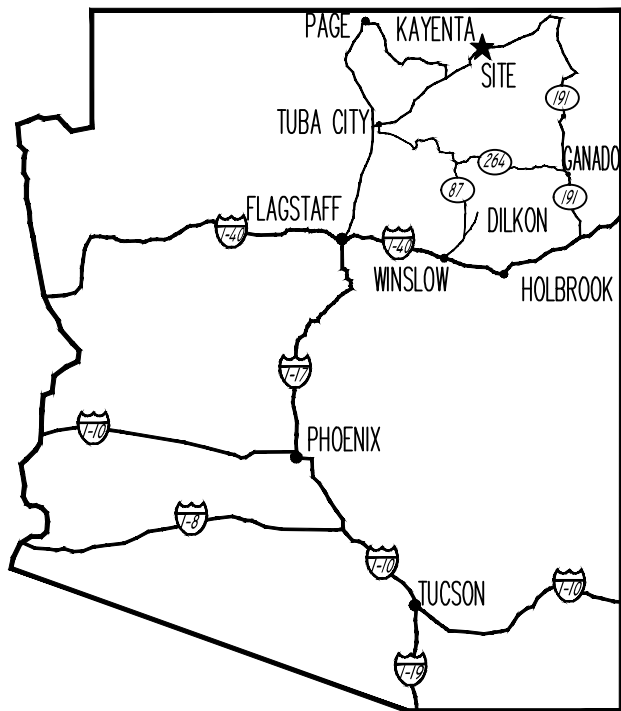
MEASURED ON A SAMPLE COMPACTED TO APPROXIMATELY 95 PERCENT OF THE ASTM D698 MAXIMUM DRY DENSITY AT ABOUT 3 PERCENT BELOW OPTIMUM WATER CONTENT. THE SAMPLE IS CONFINED UNDER A 100 PSF SURCHARGE AND SUBMERGED.

PAVING NOTES:

1. EXACT POINT OF MATCHING TERMINATION AND OVERLAY, IF NECESSARY, SHALL BE DETERMINED IN THE FIELD BY THE OWNER OR IS AUTHORIZED REPRESENTATIVE.
2. NO JOB WILL BE CONSIDERED COMPLETE UNTIL: A) ALL CURBS, PAVEMENTS, SIDEWALKS CATCH BASINS, STORM DRAINS, AND MANHOLES HAVE BEEN CLEANED OF ALL DIRT AND DEBRIS; B) SURVEY MONUMENTS ARE INSTALLED AND STAMPED; C) ALL FRAMES, COVERS VALVE BOXES ARE ADJUSTED TO GRADE.
3. NO PAVING CONSTRUCTION SHALL BE STARTED UNTIL ALL UTILITY LINES ARE COMPLETED AND APPROVED UNDER PROPOSED PAVING AREAS.
4. BASE COURSE WILL NOT BE PLACED UNTIL SUBGRADE HAS BEEN APPROVED BY THE OWNER OR HIS AUTHORIZED REPRESENTATIVE.
5. THE LOCATION OF ALL WATER VALVES, FIRE HYDRANTS, AND MANHOLES MUST AT ALL TIMES DURING CONSTRUCTION BE REFERENCED AND MADE ACCESSIBLE TO THE OWNER.
6. UTILITY FACILITIES IN CONFLICT WITH THIS WORK WILL BE RELOCATED BY THE PERMITEE OR THE UTILITY OWNER. THIS ACTIVITY SHALL BE COORDINATED WITH THE OWNER OF THE UTILITY TO PREVENT ANY UNNECESSARY INTERRUPTION OF SERVICE TO EXISTING CUSTOMERS.
7. EXISTING STREET NAME SIGNS, TRAFFIC SIGNS AND DEVICES SHALL BE MAINTAINED DURING CONSTRUCTION AND RELOCATED BY THE CONTRACTOR AS SHOWN ON THE APPROVED PLANS.
8. ALL CURB AND GUTTER, SIDEWALK, DRIVEWAYS, AND SIDEWALK RAMPS SHALL BE CONSTRUCTED ON A MINIMUM 3 INCHES (80 mm) OF AGGREGATE BASE COURSE (ABC). THE ABC SHALL BE CONSTRUCTED PER MAG SECTION 310 AND SHALL BE COMPACTED TO 95% RELATIVE DENSITY.

NEW BUS ROUTE CONSTRUCTION NOTES:

1. ALL WORKMANSHIP, TESTING, AND MATERIALS SHALL CONFORM TO THE KAYENTA TOWNSHIP SPECIFICATIONS FOR CONSTRUCTION MANUAL AND THE SUPPLEMENTAL SPECIFICATIONS FOR THIS PROJECT.
2. ALL DITCHES/DRAINAGE CHANNELS SHALL BE STAKED AND GRADED TO DRAIN.
3. THE LOCATIONS OF UTILITIES SHOWN ON THESE PLANS ARE APPROXIMATE AND ARE SHOWN TO AID THE CONTRACTOR. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS DURING CONSTRUCTION TO PREVENT DAMAGE TO ANY UTILITIES. PRIOR TO THE START OF ANY CONSTRUCTION, THE CONTRACTOR MUST HAVE THE LOCAL UTILITIES COMPANIES BLUE-STAKE ALL UTILITY LINES WITHIN THE IMMEDIATE AREA.
4. THE CONTRACTOR SHALL ASSUME FULL RESPONSIBILITY AND EXPENSE FOR DISPOSAL OF WASTE MATERIAL AND/OR CONSTRUCTION DEBRIS.
5. IN NO CASE SHALL ANY EMBANKMENT MATERIAL BE PLACED ON FROZEN, MUDDY, OR UNSTABLE NATURAL GROUND.
6. THE CONTRACTOR SHALL BE REQUIRED TO ADEQUATELY FINISH THE EXISTING NATIVE MATERIAL AND/OR IMPORT NEW MATERIAL. (IF APPLICABLE) TO THE GRADES AND DETAILS SHOWN ON THESE PLANS.
7. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE AN ADEQUATE TRAFFIC CONTROL PLAN WHERE APPLICABLE AND IN ACCORDANCE WITH THE MUTCD (MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES) 2003 EDITION.
8. WATER PROOF SEALANT SHALL BE APPLIED TO ALL EXPOSED JOINTS OF ANY STRUCTURES JOINING AT A COMMON JOINT.
9. CONTRACTOR SHALL PROVIDE STORMWATER POLLUTION PLAN AND MANUAL UTILIZING BIO-SOCKS AT R.O.W.



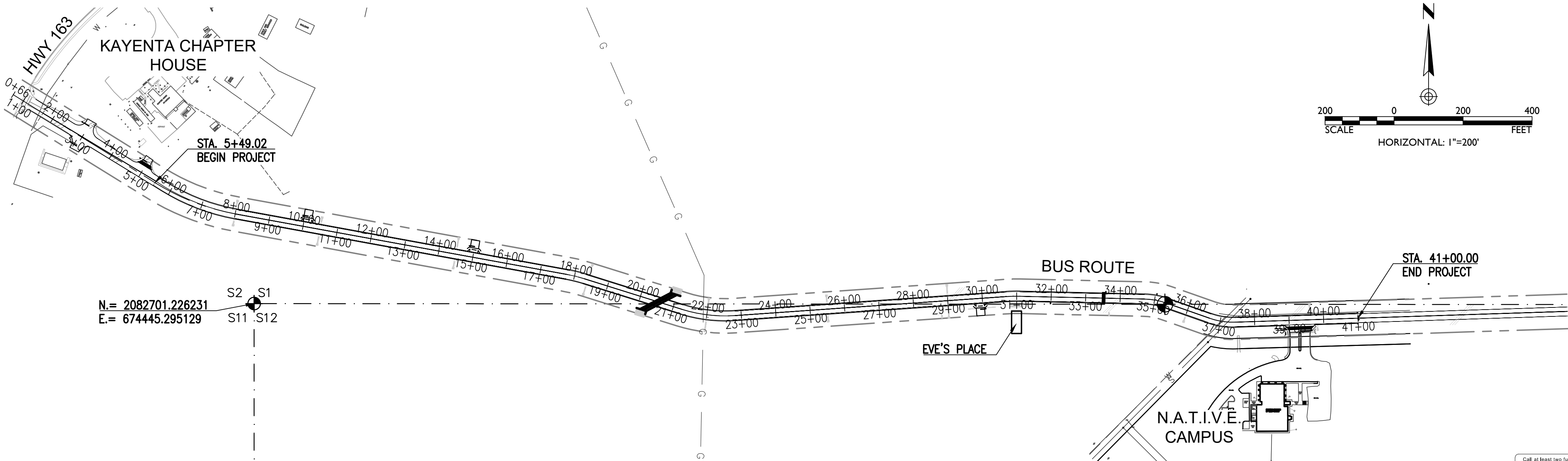
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LEGEND

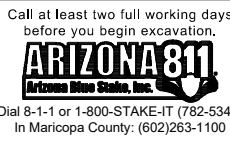
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-.-.-	SECTION LINE
+	SECTION CORNER - 1/4 CORNER
---	PROPOSED ROAD CENTER LINE
---	EXISTING AC PAVEMENT
---	EXISTING CONTOUR - MAJOR
---	EXISTING CONTOUR - MINOR
---	EXISTING SEWER
---	EXISTING WATER
---	EXISTING ELECTRIC
---	EXISTING CMP
---	EXISTING AC

SHEET INDEX

CS	COVER/NOTES
PP01-PP04	PLAN & PROFILES
PP05	DETAILS I
PP06	DETAILS II
PP07	PEDESTRIAN CROSSING DETAILS



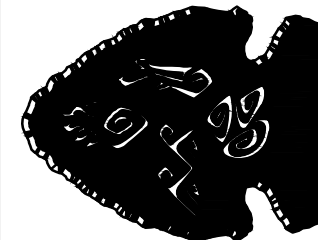
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1685 S. SAN TODARO PLACE  
TUCSON, ARIZONA 85713  
PHONE: 520-822-7702  
FAX: 520-777-3438



KAYENTA TOWNSHIP BUS ROUTE  
KAYENTA, AZ  
ROADWAY AND DRAINAGE  
IMPROVEMENT PLANS

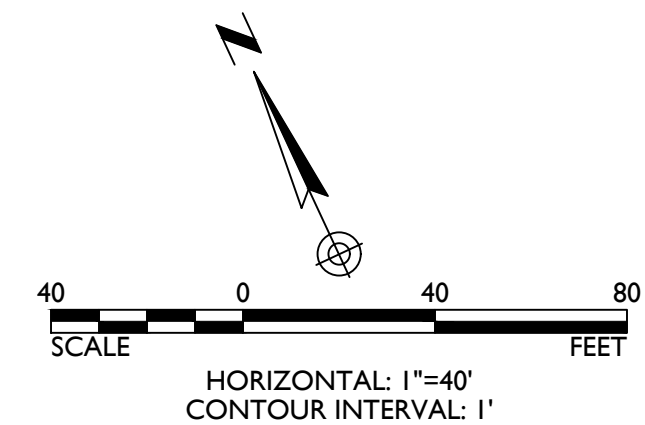
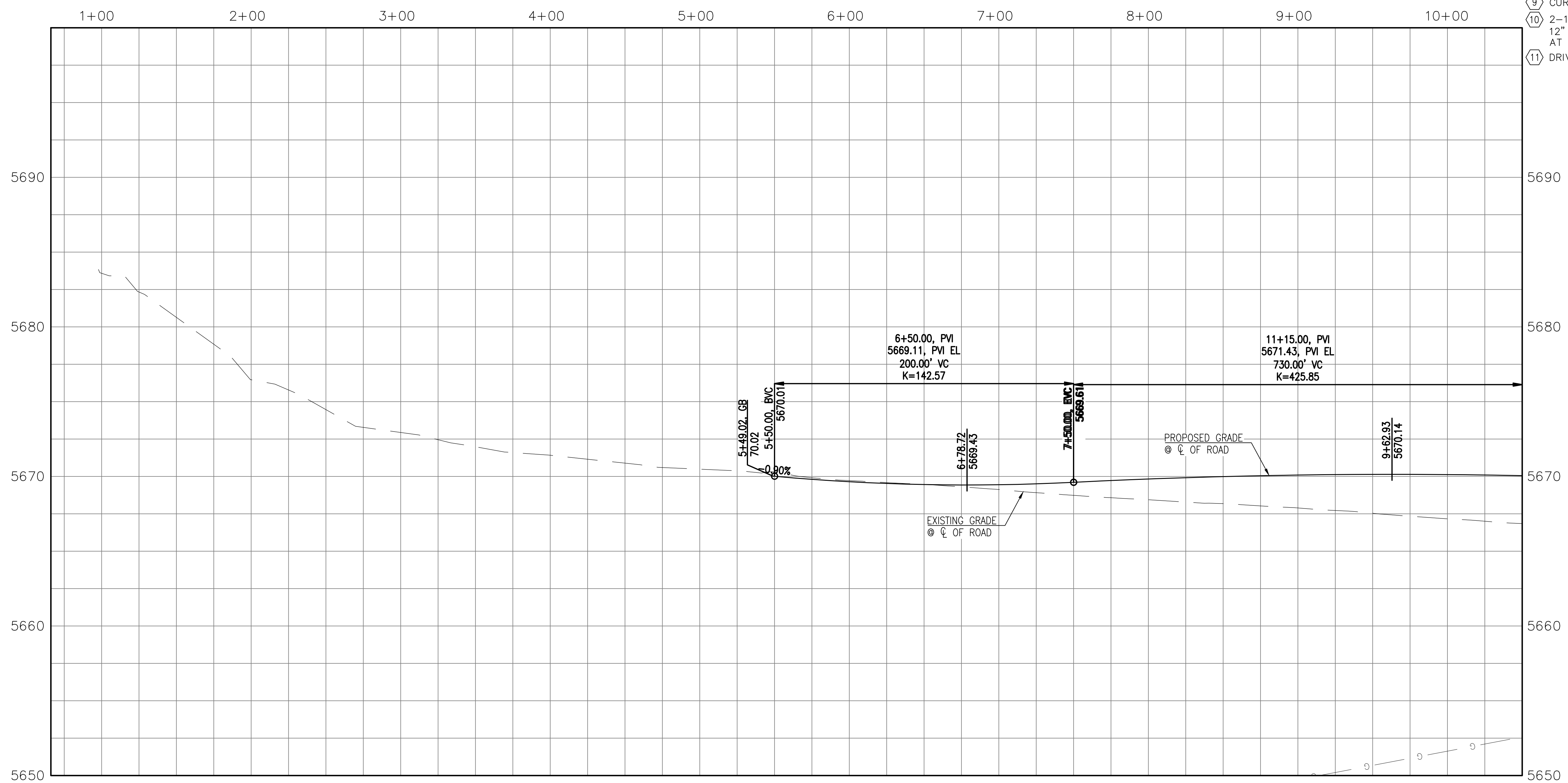
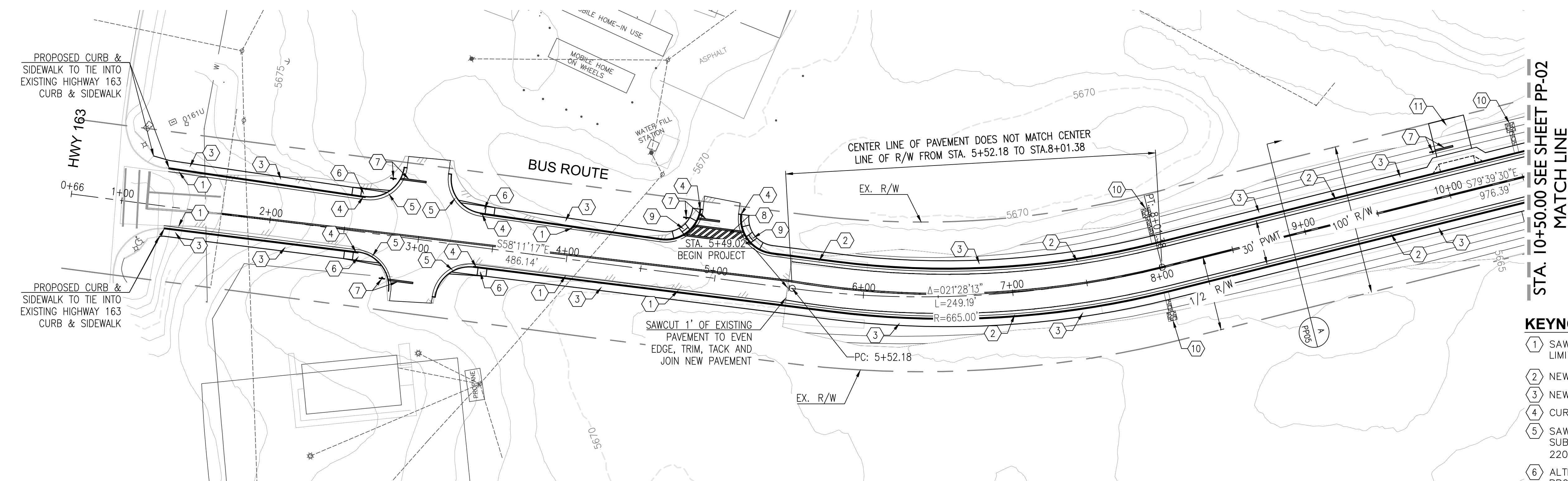


PROJECT NO.: 2017-235  
DATE: 8-07-17  
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DRAWN BY: BEZ  
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TUCSON, ARIZONA 85713  
PHONE: 520-822-7702  
FAX: 520-777-3438

# KAYENTA TOWNSHIP BUS ROUTE

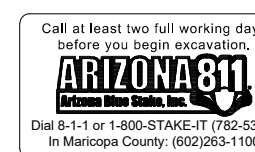
## KAYENTA, AZ

### ROADWAY AND DRAINAGE IMPROVEMENT PLANS

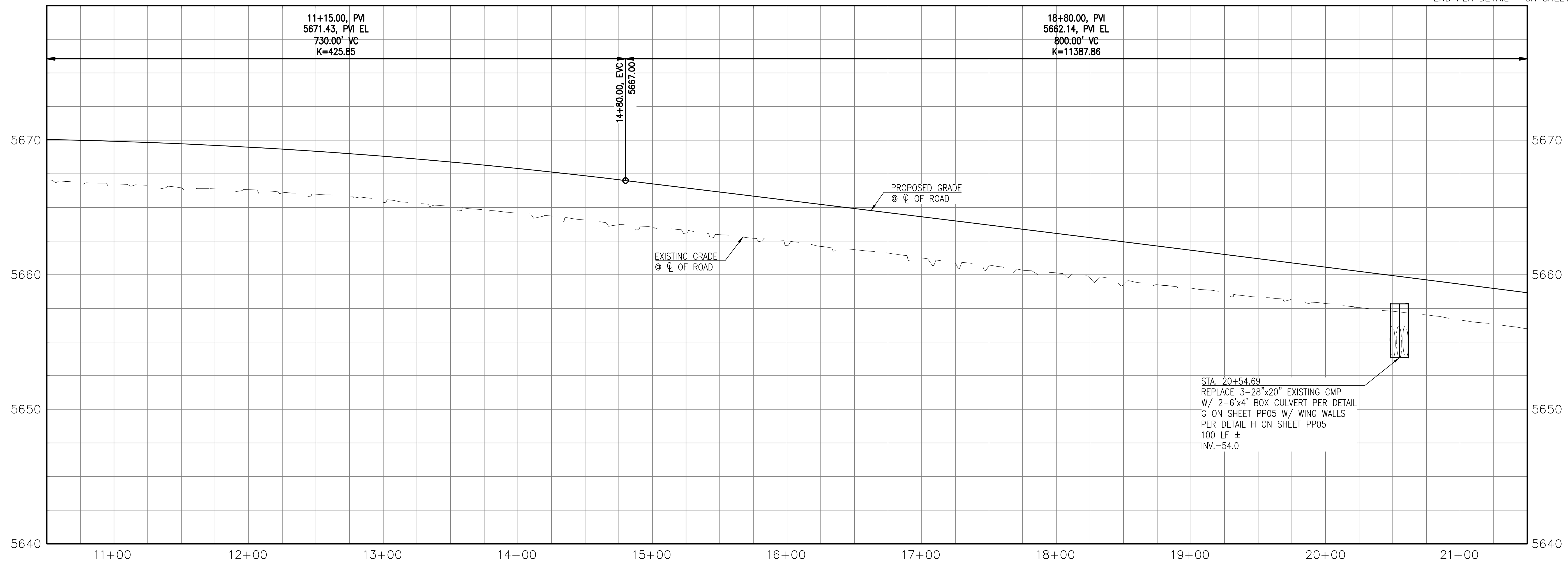
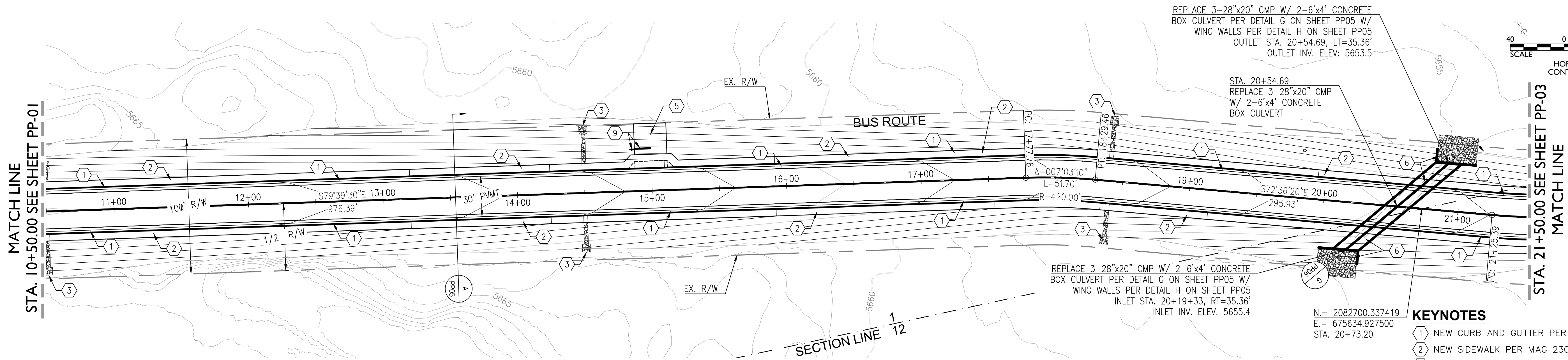


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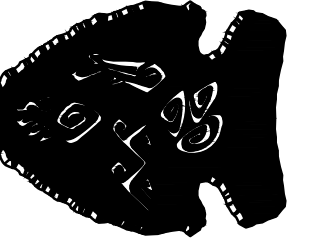


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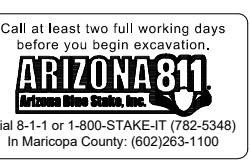


# KAYENTA TOWNSHIP BUS ROUTE KAYENTA, AZ ROADWAY AND DRAINAGE IMPROVEMENT PLANS

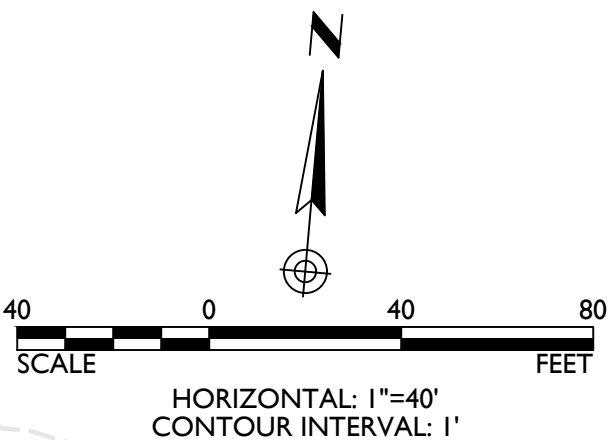


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PHONE: 520-822-7702  
FAX: 520-777-3438



# KAYENTA TOWNSHIP BUS ROUTE KAYENTA, AZ ROADWAY AND DRAINAGE IMPROVEMENT PLAN



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**PP03**

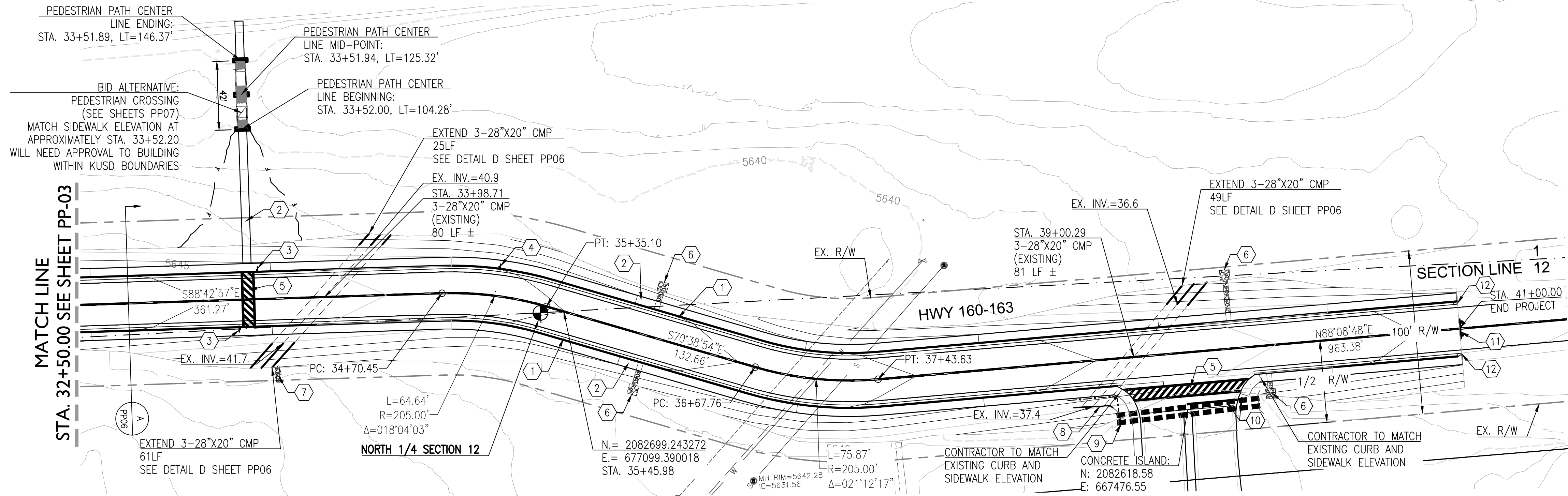


Call at least two full working days  
before you begin excavation.

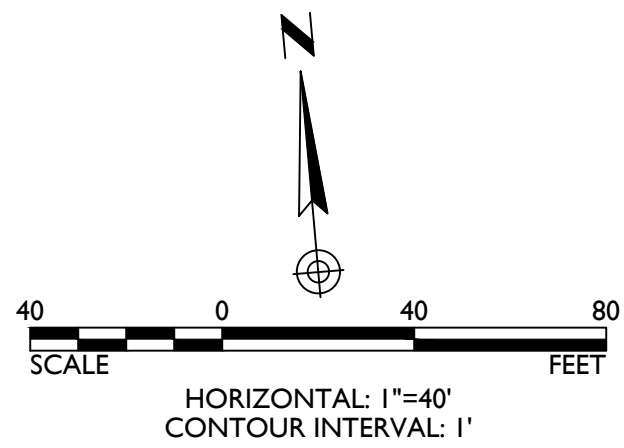
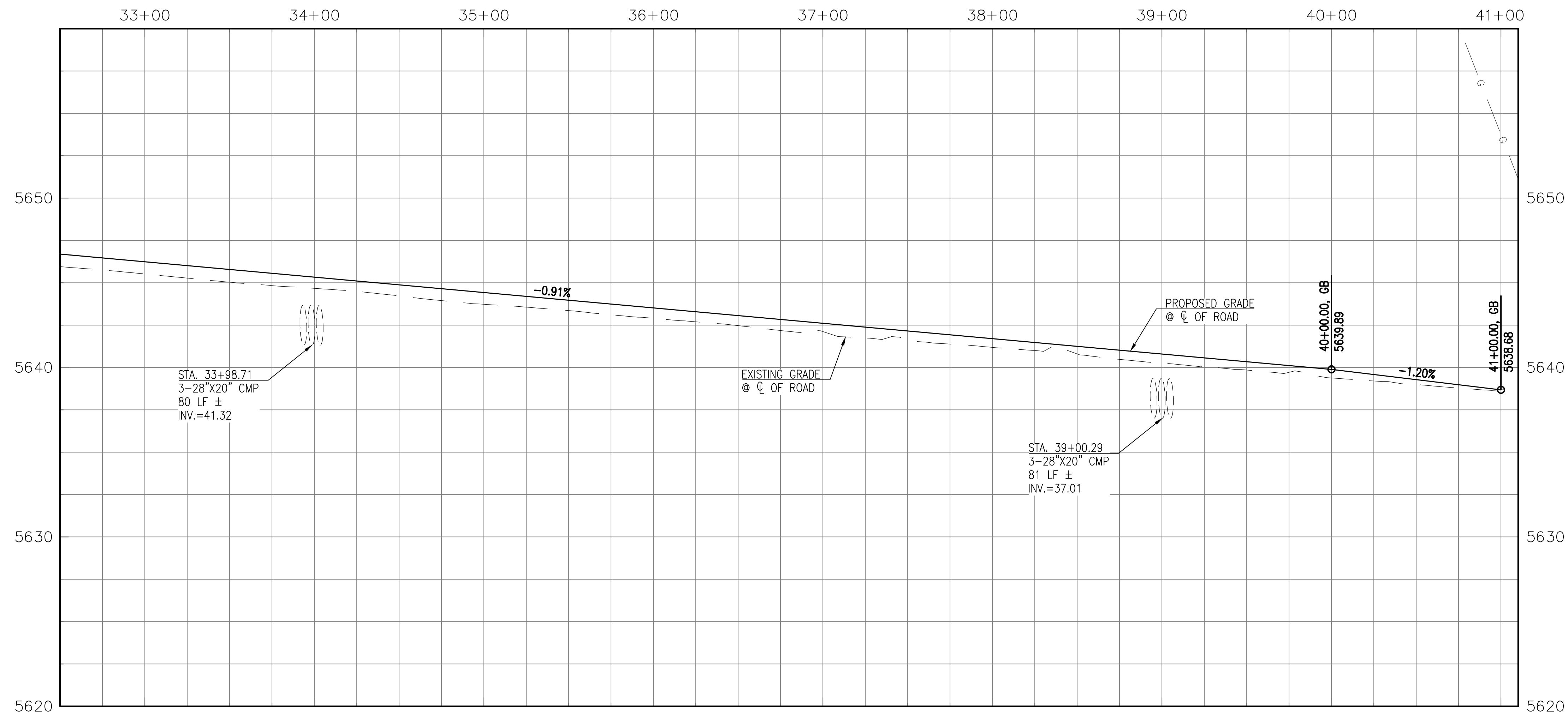
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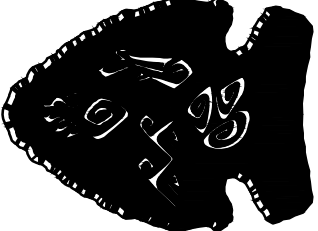
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TUCSON, ARIZONA 85713  
PHONE: 520-822-7702  
FAX: 520-777-3438



# KAYENTA TOWNSHIP BUS ROUTE KAYENTA, AZ ROADWAY AND DRAINAGE IMPROVEMENT PLANS



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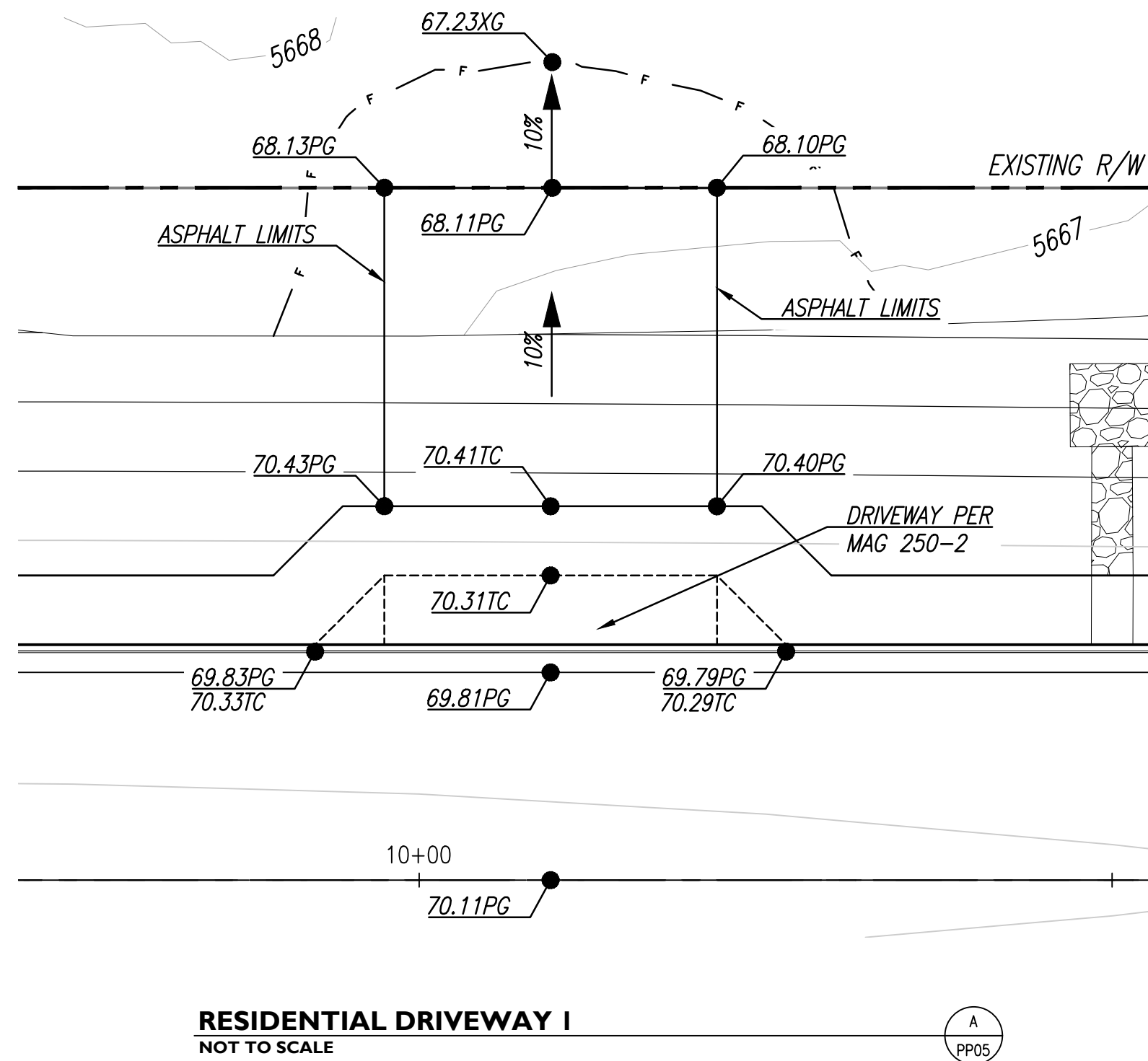
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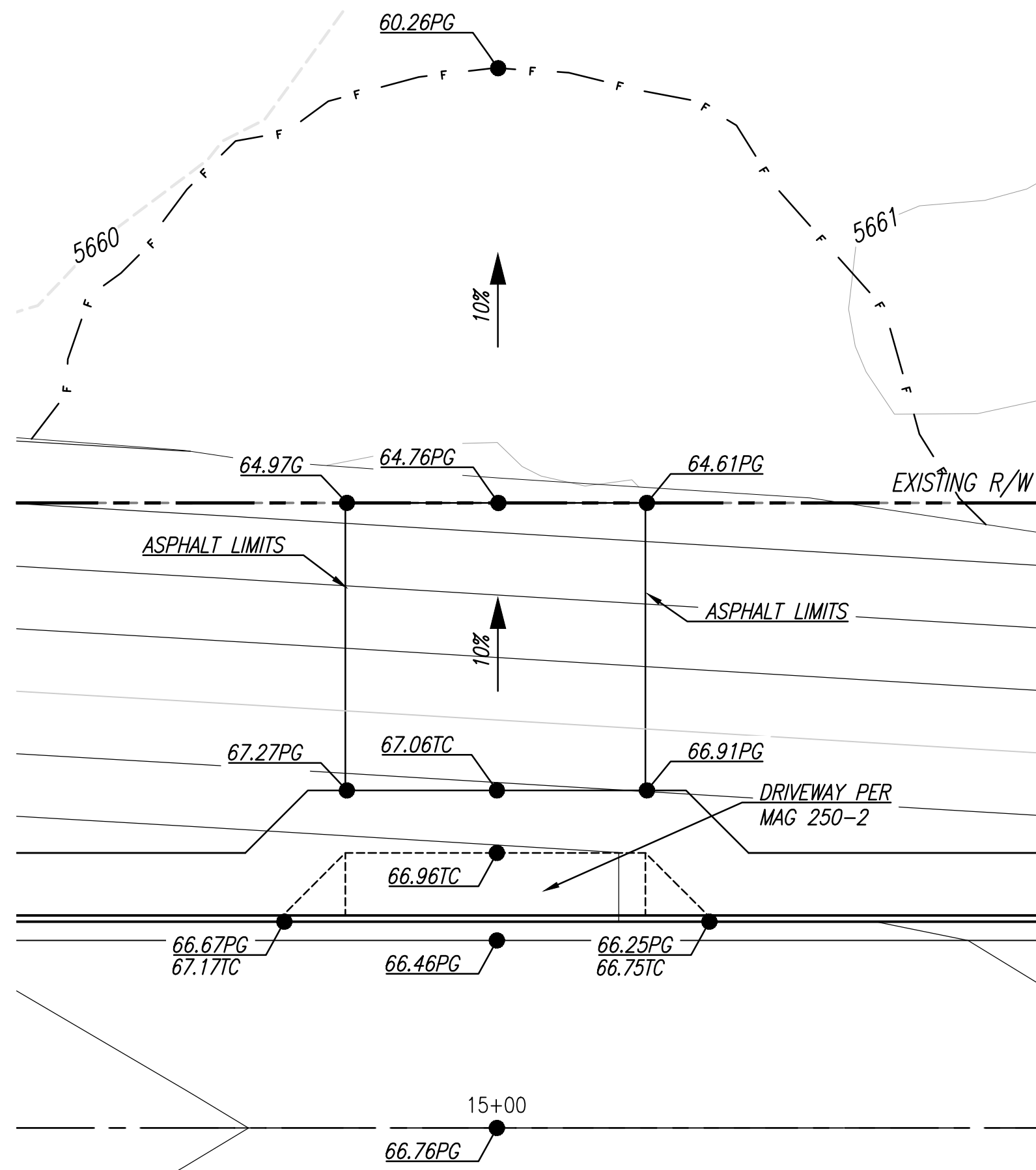
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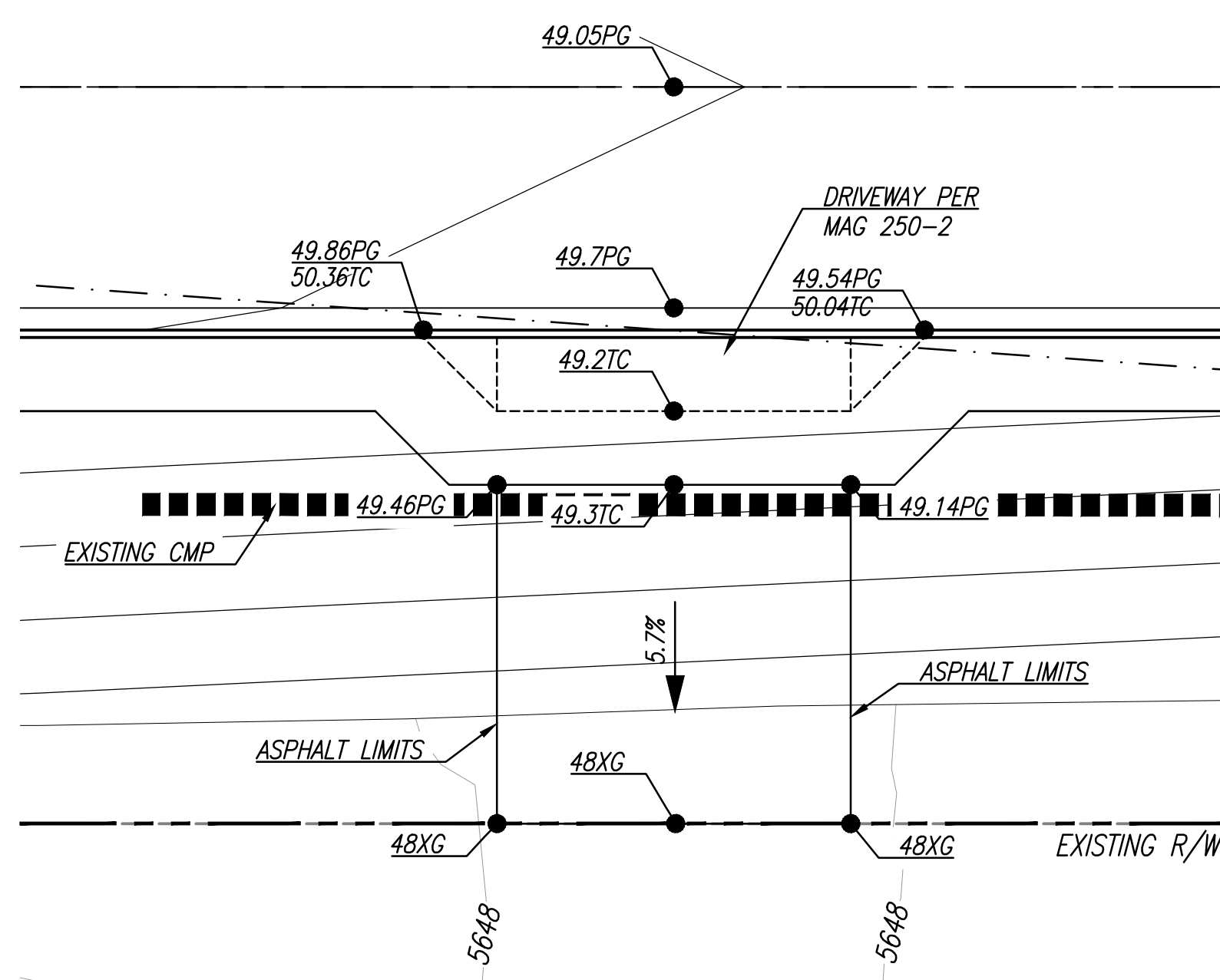
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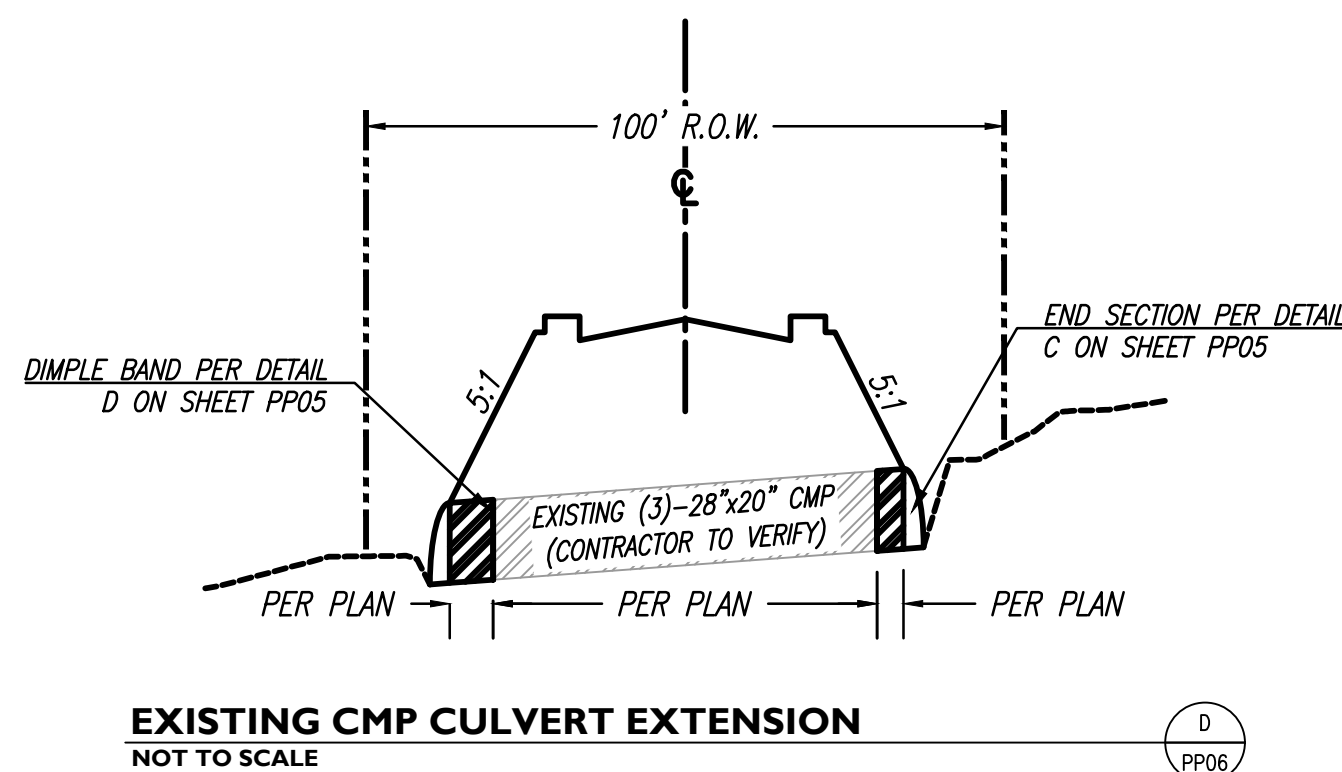
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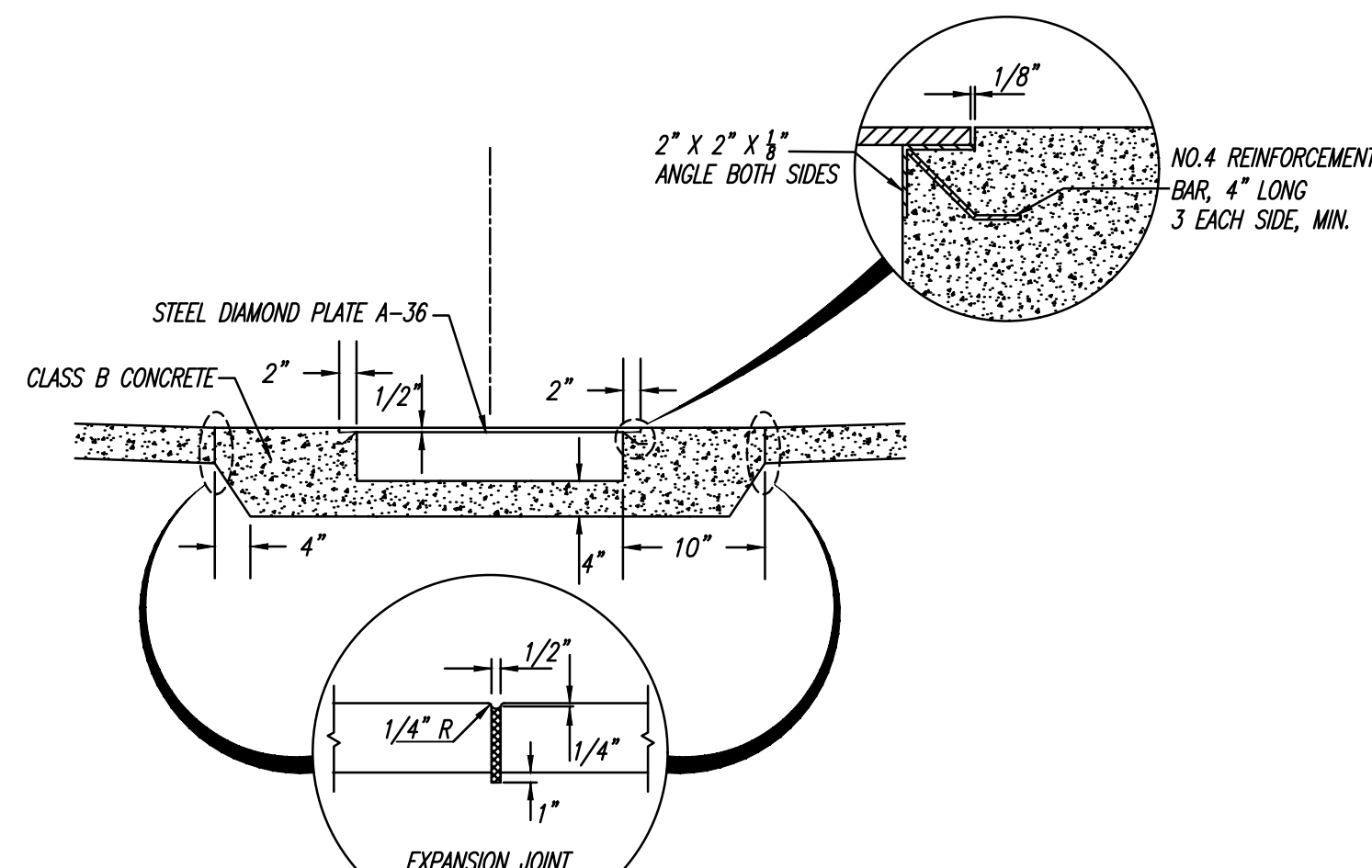
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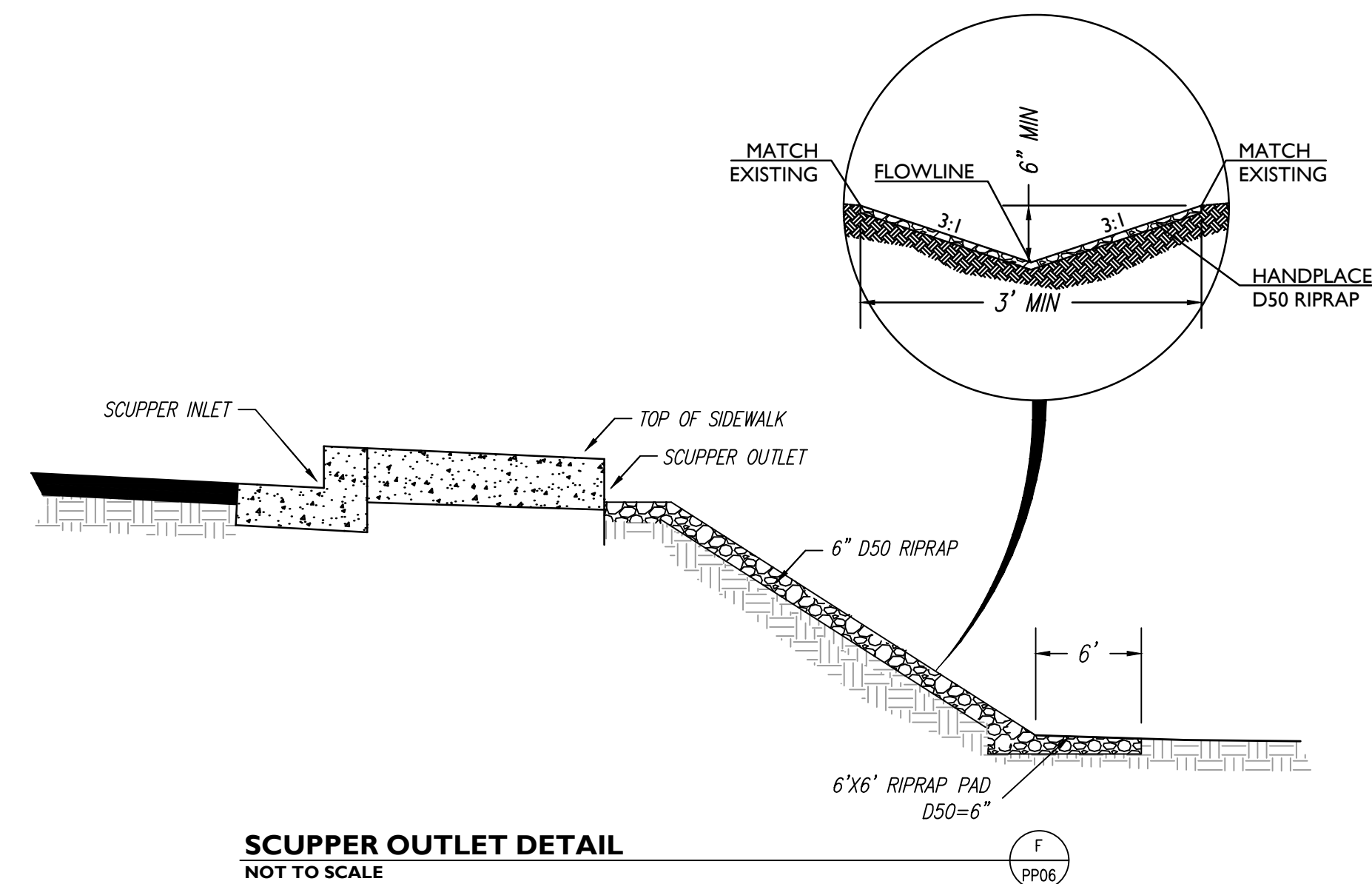
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PP05



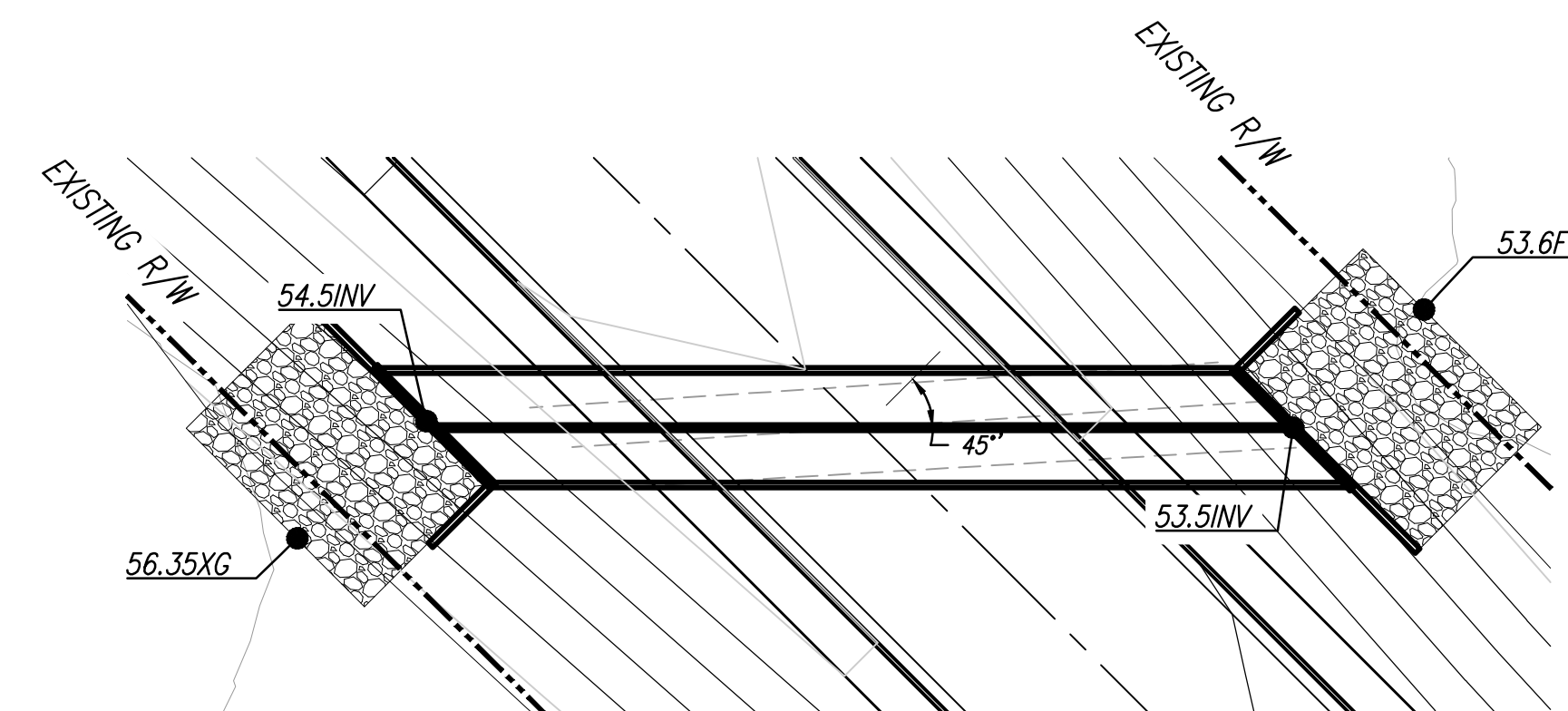
18" SIDEWALK SCUPPER DETAIL  
NOT TO SCALE

E  
PP06



SCUPPER OUTLET DETAIL  
NOT TO SCALE

F  
PP06



BOX CULVERT PLAN & PROFILE  
NOT TO SCALE

G  
PP06



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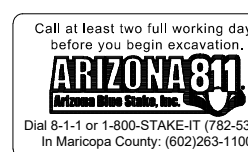


# KAYENTA TOWNSHIP BUS ROUTE KAYENTA, AZ ROADWAY AND DRAINAGE IMPROVEMENT PLANS



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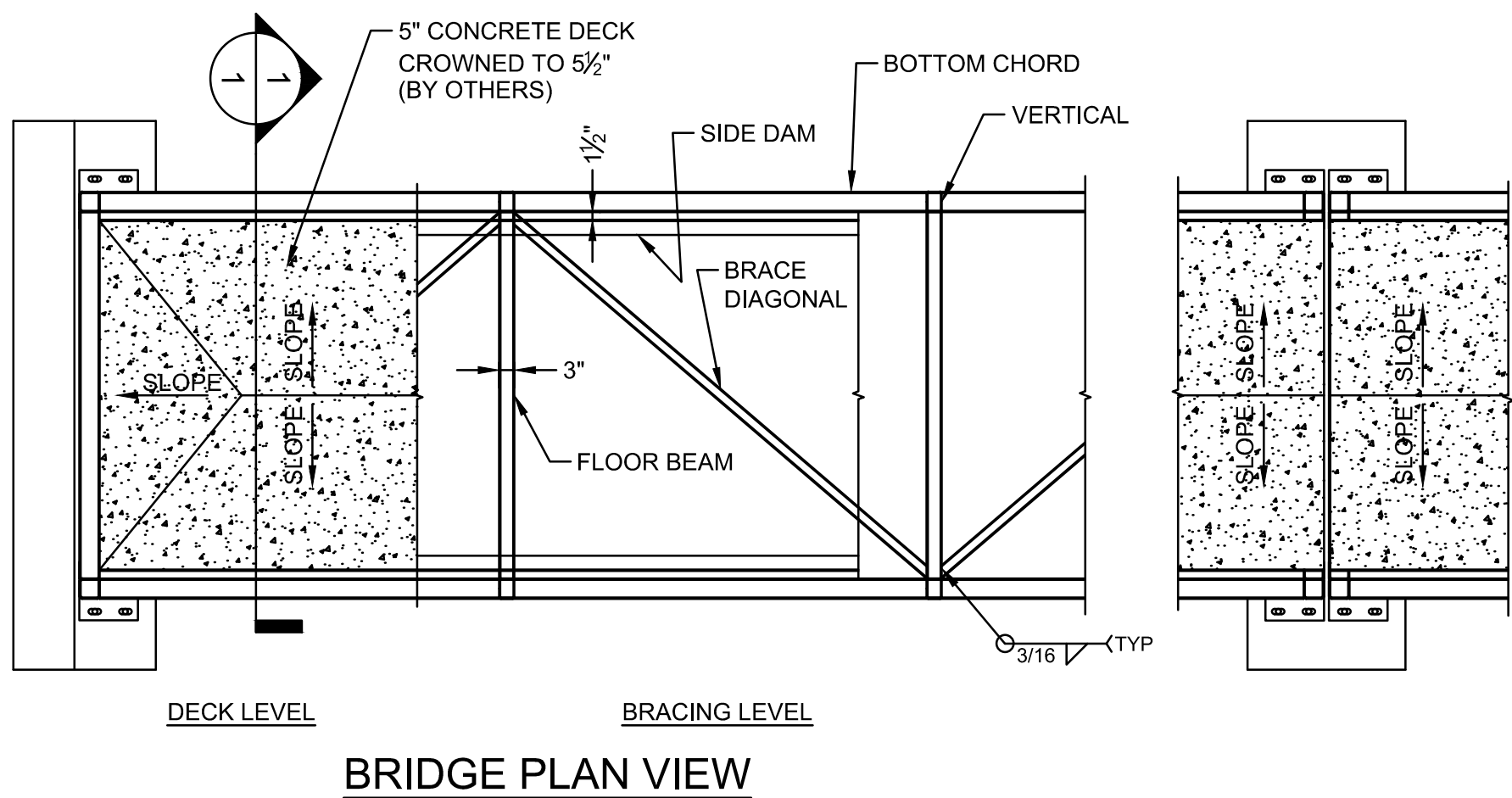
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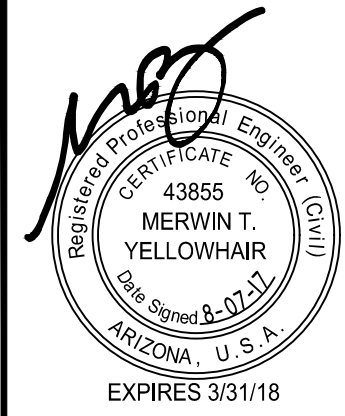
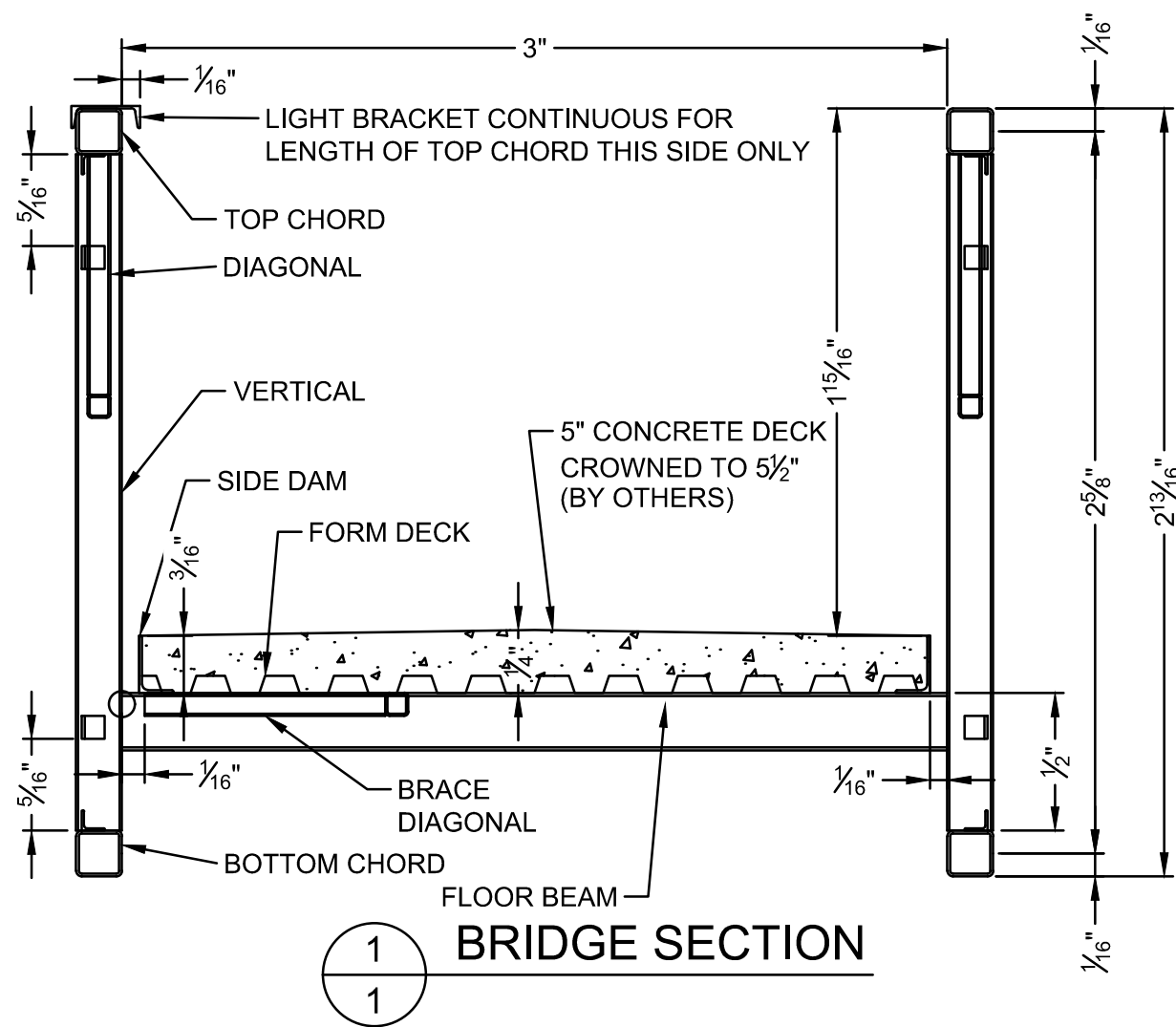
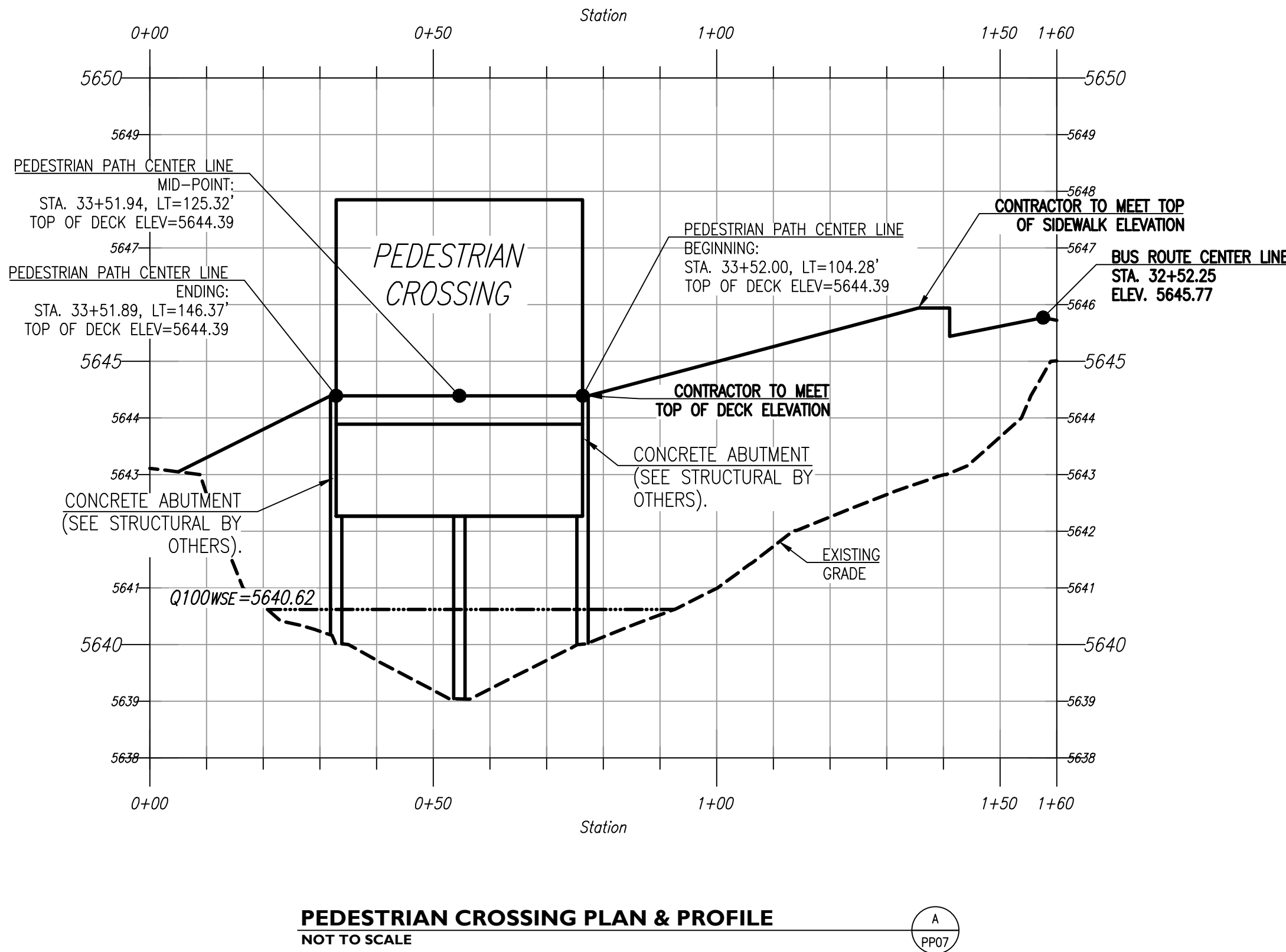
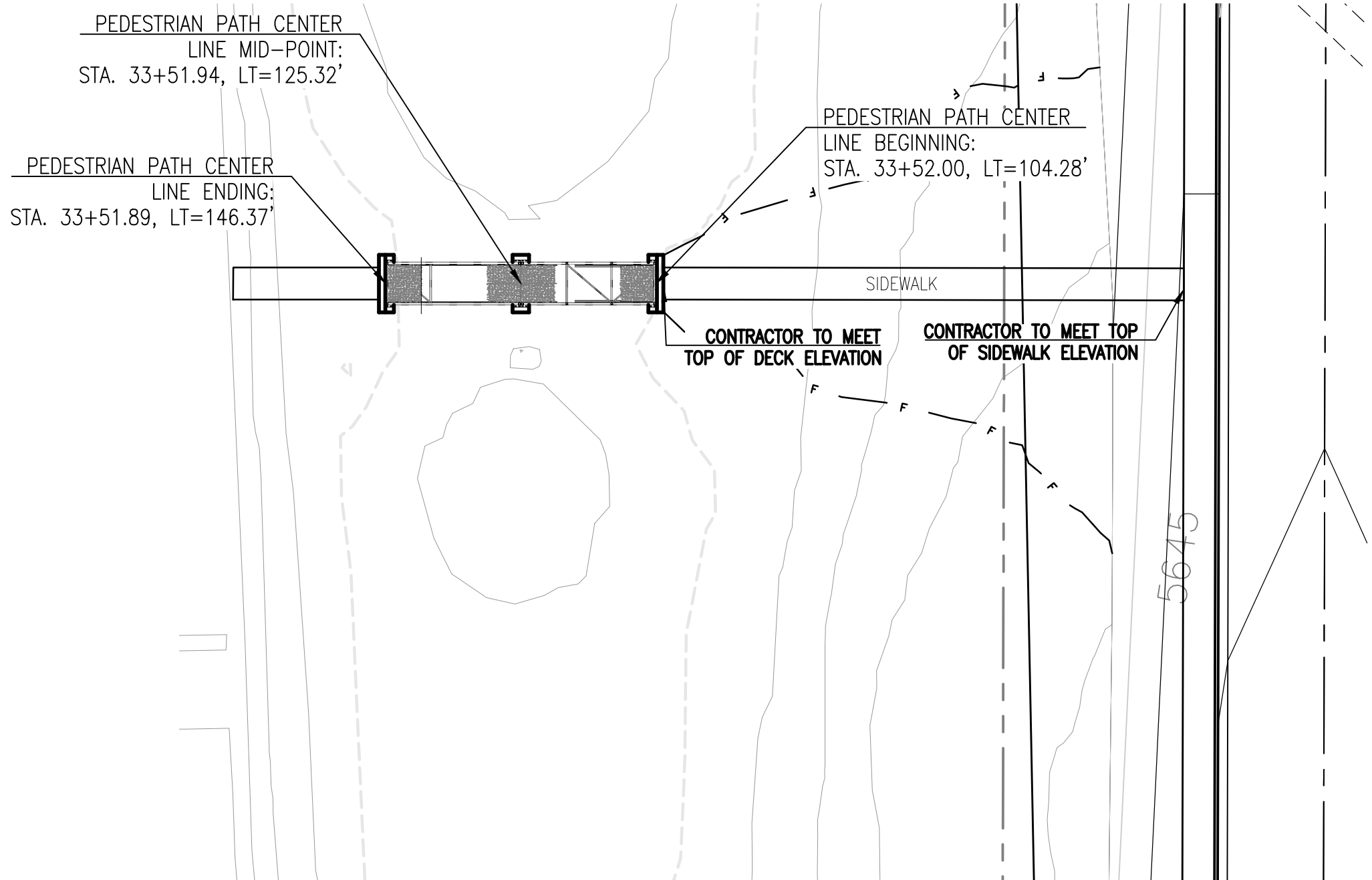


GENERAL NOTES

1. DESIGN STRESSES ARE IN ACCORDANCE WITH THE MANUAL OF STEEL CONSTRUCTION FOR ALLOWABLE STRESS DESIGN AS ADOPTED BY THE AMERICAN INSTITUTE OF STEEL CONSTRUCTION (AISC).
2. BRIDGE MEMBERS ARE FABRICATED FROM HIGH STRENGTH, LOW ALLOY, ENHANCED ATMOSPHERIC CORROSION RESISTANT ASTM A847 COLD-FORMED WELDED SQUARE AND RECTANGULAR TUBING, AND ASTM A588, ASTM A606, OR ASTM A709-50W PLATE AND STRUCTURAL SHAPES (Fy=50,000 PSI).
3. CONCRETE DECK: GALVANIZED FORM DECK SUPPLIED BY CONTECH. CONCRETE, REINFORCING AND EXPANSION MATERIAL SUPPLIED BY OTHERS. CONTRACTOR TO CONTACT CONTECH STRUCTURAL ENGINEER FOR DETAILS.
4. THE GAS METAL ARC WELDING PROCESS OR FLUX CORED ARC WELDING PROCESS WILL BE USED. WELDING TO BE IN ACCORDANCE WITH AWS D1.1.
5. ALL TOP AND BOTTOM CHORD SHOP SPLICES TO BE COMPLETE PENETRATION TYPE WELDS. WELD BETWEEN TOP CHORD AND END VERTICAL SHALL BE AS DETAILED BY CONTECH STRUCTURAL ENGINEER.
6. UNLESS OTHERWISE NOTED, WELDED CONNECTIONS SHALL BE FILLET WELDS (OR HAVE THE EFFECTIVE THROAT OF A FILLET WELD) OF A SIZE EQUAL TO THE THICKNESS OF THE LIGHTEST GAGE MEMBER IN THE CONNECTION. WELDS SHALL BE APPLIED AS FOLLOWS:
- A. BOTH ENDS OF VERTICALS, DIAGONALS, AND FLOOR BEAMS SHALL BE WELDED ALL AROUND.
- B. BRACE DIAGONALS WILL BE WELDED ALL AROUND.
- C. MISCELLANEOUS NON-STRUCTURAL MEMBERS WILL BE STITCH WELDED TO THEIR SUPPORTING MEMBERS.
7. BRIDGE DESIGN WAS ONLY BASED ON COMBINATIONS OF THE FOLLOWING LOADS WHICH WILL PRODUCE MAXIMUM CRITICAL MEMBER STRESSES.
- A. 85 PSF UNIFORM LIVE LOADING ON THE FULL DECK AREA OR ONE 6,000 LB VEHICLE LOAD. THE LOAD SHALL BE DISTRIBUTED AS A FOUR-WHEEL VEHICLE WITH 60% OF THE LOAD ON THE REAR WHEELS. THE WHEEL TRACK WIDTH OF THE VEHICLE SHALL BE 4'-0" AND THE WHEEL BASE SHALL BE 5'-6". THE VEHICLE SHALL BE POSITIONED SO AS TO PRODUCE THE MAXIMUM STRESSES IN EACH MEMBER, INCLUDING DECKING.
- B. 25 PSF WIND LOAD ON THE FULL HEIGHT OF THE BRIDGE, AS IF ENCLOSED.
- C. 20 PSF UPWARD FORCE APPLIED AT THE WINDWARD QUARTER POINT OF THE TRANSVERSE BRIDGE WIDTH (AASHTO 3.8.2).
8. CLEANING: ALL EXPOSED SURFACES OF STEEL SHALL BE CLEANED IN ACCORDANCE WITH STEEL STRUCTURES PAINTING COUNCIL SURFACES PREPARATION SPECIFICATIONS NO. 7 BRUSH-OFF BLAST CLEANING. SSPC-SP7-LATEST EDITION.



- NOTE:
1. THIS BRIDGE IS BEING PROVIDED AS ORDERED WITH NO SAFETY SYSTEM (SAFETY RAILS, PICKETS OR FENCING) ON THE SIDE TRUSSES. THE SPECIFIER AND/OR THE OWNER, BY APPROVAL OF THESE DRAWINGS, ACCEPTS ALL LIABILITY ASSOCIATED WITH THE LACK OF AN "ADEQUATE" SAFETY SYSTEM ON THE STRUCTURE. AN "ADEQUATE" SYSTEM IS USUALLY DEFINED AS ONE WHICH PREVENTS A 4" SPHERE FROM PASSING THROUGH THE TRUSS; HOWEVER, THIS REQUIREMENT MAY VARY IF CONTROLLED BY OSHA OR AASHTO REGULATIONS OR SOME OTHER LOCAL, STATE OR NATIONAL BUILDING CODE.
2. THESE BRIDGE SCHEMATICS ARE TO BE USED BY THE CONTRACTOR TO OBTAIN DETAILED ENGINEERED STRUCTURAL DRAWINGS COMPLETE WITH STRUCTURAL ENGINEERING STAMP. THE PEDESTRIAN BRIDGE SIMILAR OR EQUAL TO THE KIND PROVIDED BY CONTECH WILL BE REVIEWED AT BIDDING. THE PLAN AND PROFILE SECTION ARE MEANT TO PROVIDE BIDDING ASSISTANCE AND NOTHING MORE.



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REVISIONS		description	date
no.	1	KAYENTA TOWNSHIP REVIEW	08-04-17

**ARROWHEAD  
ENGINEERING, INC.**

1685 S. SAN TODARO PLACE  
TUCSON, ARIZONA 85713  
PHONE: 520-822-7702  
FAX: 520-777-3438

KAYENTA TOWNSHIP BUS ROUTE  
KAYENTA, AZ  
ROADWAY AND DRAINAGE  
IMPROVEMENT PLANS



PROJECT NO.: 2017-235  
DATE: 8-07-17  
DESIGNED BY: MTY  
DRAWN BY: BEZ  
APPROVED BY:

SHEET NO:  
**PP07**

